

Public Input	Effects Residents or Businesses
Citywide	
Emery Go-Round routes should have metered parking to dissuade commuter parking.	
Long-term, overnight parking for residents who do not have off-street parking should be considered.	
Parking regulations need to be enforced.	
Residential permit holders should park for free at metered parking spaces on street cleaning days.	
If parking demand is caused by nonresidential development, parking costs should be paid for by developers.	
Financing	
Increase investments in encouraging people to walk, bike, or take public transi. Consider revenue use for park and ride or garages.	
Increase enforcement by adding dedicated police staff.	
Encourage individuals without a car to live in Emeryville or to decrease car ownership for existing residents through improving transit in Emeryville.	
Parking revenue should be used to increase night-time surveillance (e.g., break-ins).	
Parking revenue should be used to provide transit subsidies to all or selected residents and to enhance transit services (e.g., extended hours or routes).	
Parking revenue should be used for parks, building requirements, and constructing new parking.	
Improve and add street lighting to encourage walking to transit and other areas (e.g., Doyle between 59th and Powell).	
North Hollis/Doyle	
Residential and business permits are needed on both sides of 62nd Street.	
Businesses should not be able to park on residential streets east of Doyle Street.	Businesses
Provide residential permits for Oakland residents on Vallejo Street.	Residents
Development construction workers should not be able to park on the street and should be enforced.	Businesses
Eliminate the 2-hour limit on 62nd between Hollis and Horton (across the street from Hollis Complex) because there are more LW than parking on north side.	
Allow free 4-hour parking instead of 2-hour parking to benefit workers.	Businesses
Consider residents who don't have off-street parking options, households with more than 2 cars (with teens), and who work from home.	Residents
Incentivize businesses to encourage employees to park in garages.	Businesses
There should be no parking changes on 67th Street until needed.	Businesses
Peninsula	
Allow residential permits on Powell Street for Watergate residents and visitors of residents (e.g., nannies, eldercare workers, etc.).	Residents
Parking at the marina should be free for all, or free for those with a residential permit.	Residents
Parking at the marina should have time limits to prevent all-day commuter parking.	
North Bayfront	
Entire area should have short-term metered parking.	
South side of Haruff Avenue should be mid- or short-term parking to serve business customers.	Businesses

South of Powell and Triangle

Meters on Adeline Street should be a RPP area instead.	Residents
Senior Center parking should have longer time limits (3-4 hours).	Businesses
ECCL has designated staff parking/permits - how does the Plan incorporate existing permitting?	Businesses
Parking limits and increased enforcement should be considered for evening hours, especially near the Senior Center and in the Triangle neighborhood.	Residents
Parking without a permit should only be allowed for 30 minutes, especially near the SPA.	
Business permits should not be allowed in the Triangle neighborhood.	Businesses
Construct a new park-and-ride lot.	
Streets adjacent to the Sherwin Williams development should be designated for permit parking instead of metered parking.	Residents
Add a permit area to Park Avenue Districts.	
Horton Street near the Sherwin Williams development should have more restrictive parking to allow parking availability for residents.	Residents
Permits and meters should both be designated near the artist co-op and 1500 Park Avenue.	

Permits

Permit areas should also include metered parking for employees and visitors from Monday to Friday, 8-5.	Businesses
Permits should be transferrable (e.g., placards) for better convenience to households and businesses.	
Residential and business permit holders should be allowed to park at metered spaces at no cost.	
Proposed parking pricing should consider the effects on small businesses in Emeryville.	Businesses
Residential permit costs as proposed are too high, especially for the price jump to the second permit.	Residents
Reduce costs for residential parking permits, while increasing costs for business parking permits.	Residents
Consider how to accommodate the costs for multiple permits for renter/roommate situations.	Residents
Consider the impacts and mitigation of parking in adjacent Oakland areas.	Residents
Visitor permits should be convenient to obtain (e.g., online).	Residents
Business permits should not be available to businesses with revenues over \$2M per year.	Businesses
Price the cost of permits depending on several factors including the type of site (residential or commercial) and availability of existing off-street parking.	Residents
Business permits should only be provided for low-wage workers.	Businesses
Construction parking should be in the form of a permit for flexibility, not a designated parking space.	
All-day parking permits should be allowed for maintenance workers.	Residents
Allow more than two parking permits per dwelling unit, but raise the price significantly for the third and any subsequent permits.	Residents
Low-income residents should have free or permits at a low cost.	Residents
Business permits should have tiered pricing (e.g., by size of business) to reduce the burden for small businesses.	Businesses
Small businesses should get priority for business permits.	Businesses
Residential permits should be an option for those who live on a street with proposed metered parking.	Residents

Metered Parking

Time limits on metered parking should be based on the need for short- and long-term parking needs.	
Long-term parking pricing is too inexpensive and should be \$1 to \$1.50/hour.	
Short-term metered parking should be inexpensive for the first few hours and more expensive for hours following.	
Add metered parking in proposed permit areas, especially in the area bounded by Hollis Street, Ocean Avenue, Vallejo Street, and Powell Street.	
Increase pricing for meters so that they are more demand-based and similar to pricing in Berkeley and Oakland.	

NORTH HOLLIS/DOYLE

Recommendations

1. Significant variation of parking types among adjacent block faces to ensure the needs of visitors, employees, and residents are met
 - Short-Term and Long-Term Metered Parking
 - North Hollis/Doyle Permit Area

***Note:** Implementation for parking changes in the North Hollis/Doyle neighborhood to occur in Phase I, expected to be initiated in 2018.*

Issues

- Oakland adjacency spillover
- Employee spillover
- Occupancy exceeds 85%
- No enforcement of time limits

Parking Inventory & 10 AM Occupancy

North Hollis:

- 2,869 spaces
- 90% occupancy

Doyle:

- 822 spaces
- 87% occupancy



PENINSULA

Recommendations

- 1. Mid-term metered parking along Powell Street
 - Serves visitors to Marina Park and Bay Trail
 - Discourages long-term parking by Transbay bus riders, carpoolers, and office workers
- 2. Long-term metered parking at public marina parking lot
 - Low-cost option for park and marina visitors
 - Potential park-and-ride facility for carpoolers
 - Allow permitting system to continue for live-aboards

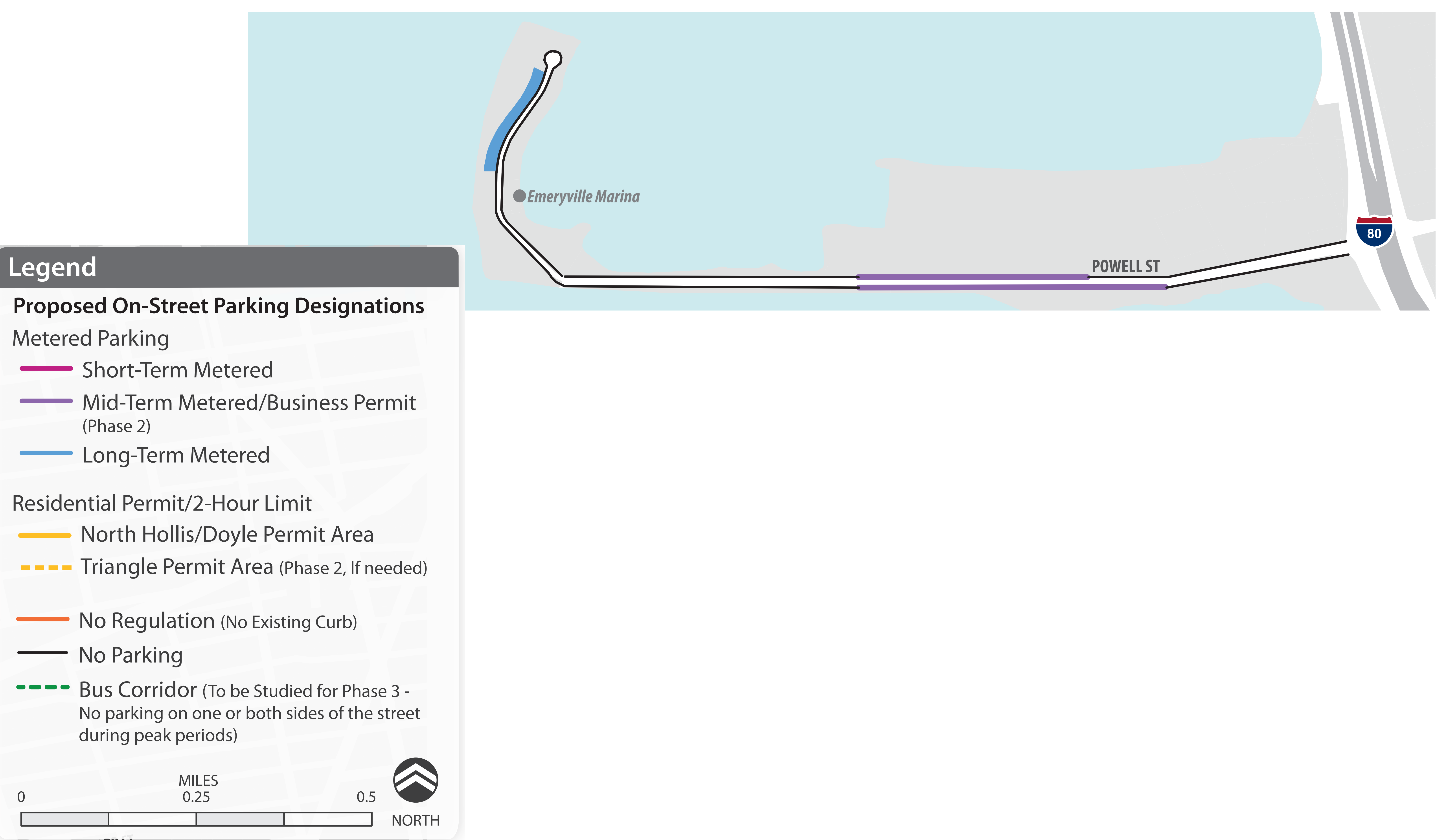
***Note:** Implementation for parking changes in the Peninsula neighborhood to occur in Phase II, expected to be implemented in 2019 or later.*

Issues

- Casual carpool
- Residential adjacency
- Marina Park/Bay Trail access
- Police access

Parking Inventory & 10 AM Occupancy

- 832 spaces in total, on-street and lots except Watergate (1,600 spaces)
- 97% occupancy on Powell Street



NORTH BAYFRONT

Recommendations

1. Short-term meters on Shellmound Street and streets in the Public Market area
2. Mid- and long-term meters on blocks further away from this activity center to serve employees
3. Multi-family residential areas bordered by mid-term metered parking to serve visitors

***Note:** Implementation for parking changes in the North Bayfront neighborhood to occur in Phase II, expected to be implemented in 2019 or later.*

Parking Inventory & 10 AM Occupancy

- 231 spaces
- 89% occupancy



SOUTH OF POWELL AND TRIANGLE

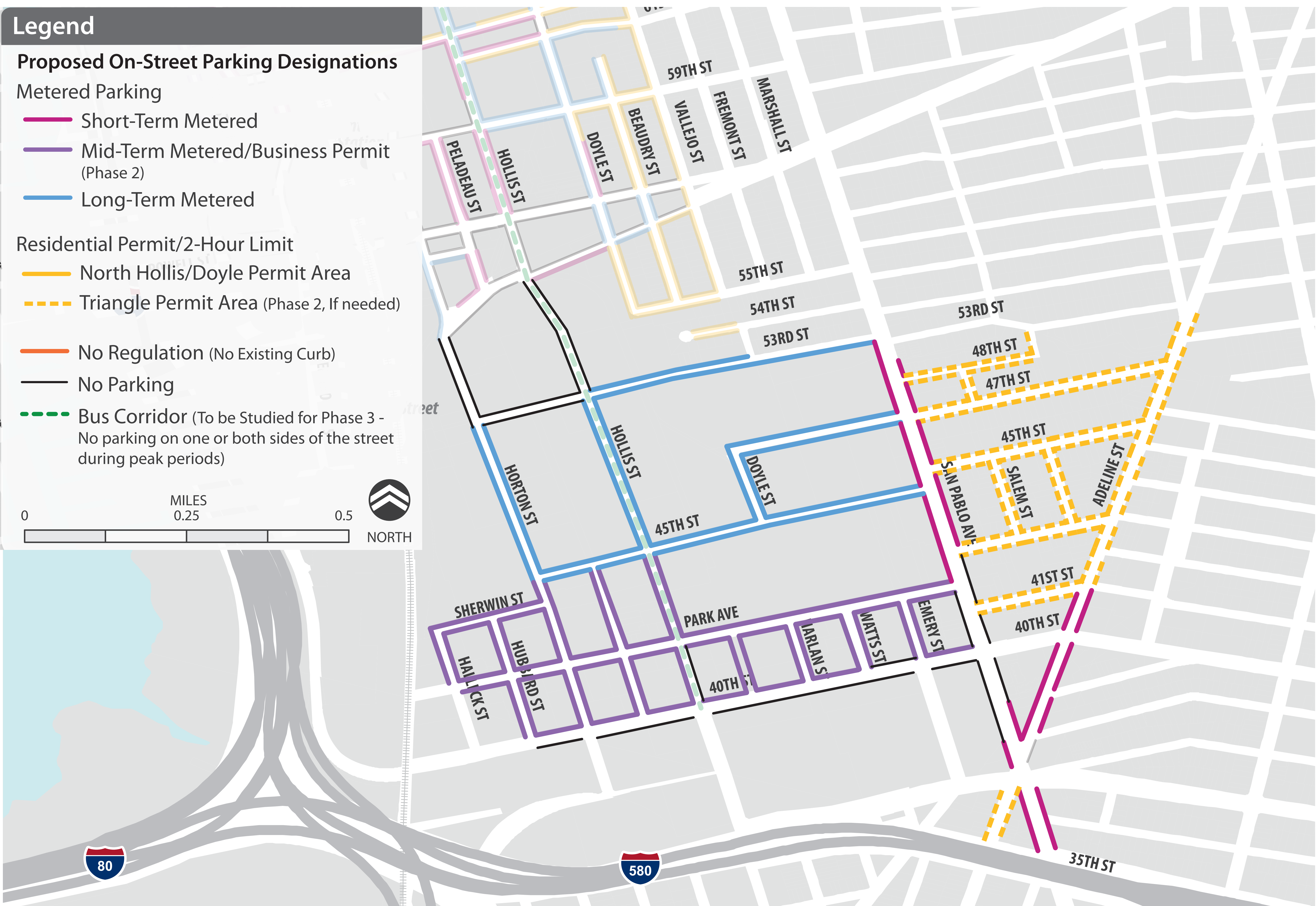
Recommendations

- 1. Mid-term meters for visitors of businesses, but dissuading park-and-ride
- 2. Long-term meters further away to serve business employees
- 3. Short-term meters on commercial segments of San Pablo Avenue and Adeline Street
- 4. Business permits for mid-term metered parking areas to serve businesses with no off-street parking
- 5. Triangle Permit Area (if needed)
- 6. Streets for Sherwin Williams development to be designated as developed

***Note:** Implementation for parking changes in the South of Powell and Triangle neighborhood to occur in Phase II, expected to be implemented in 2019 or later.*

Parking Inventory & 10 AM Occupancy

Central:	Park Avenue:	South Emeryville:	Triangle:
<ul style="list-style-type: none">• 624 spaces• 68% occupancy	<ul style="list-style-type: none">• 726 spaces• 88% occupancy	<ul style="list-style-type: none">• 105 spaces• 67% occupancy	<ul style="list-style-type: none">• 611 spaces• 71% occupancy

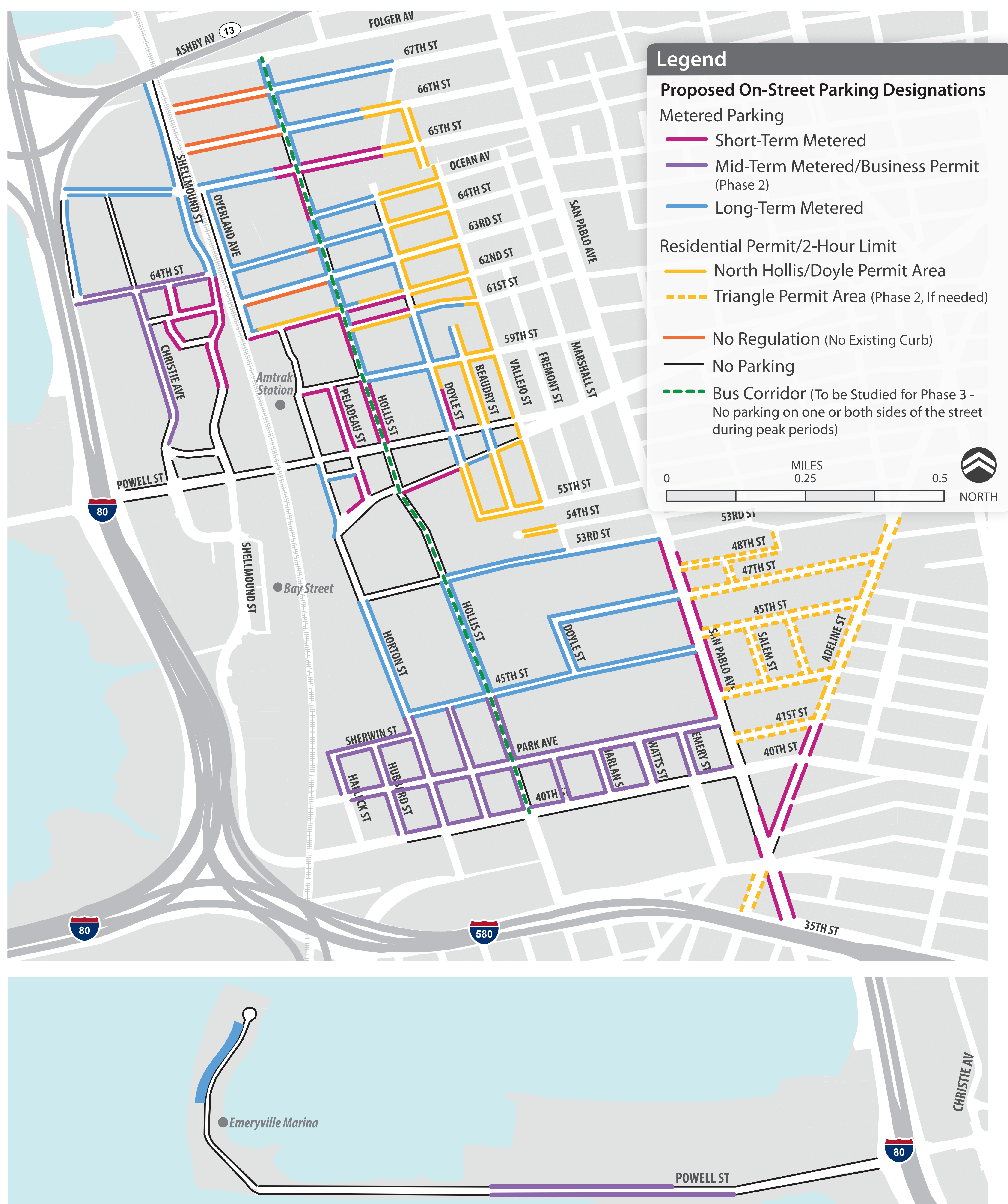


CITYWIDE

The North Hollis Bus Corridor is in consideration for Phase III, expected at earliest in 2020 or later after further study. Financial sustainability is sought throughout the system with reliability, accessibility, education, and enforcement provided consistently.

Issues

- Should parking be restricted along the North Hollis Bus Corridor in the northbound direction, southbound direction, or both?
- Should parking be restricted along the North Hollis Bus Corridor during the morning peak hour, afternoon peak hour, or both?
- How should long-term parking work with a North Hollis Bus Corridor?



FINANCING

If the City's parking program generates revenue in excess of the enforcement and operational costs of the parking program, what should the revenue be used for?

Unrestricted (support the General Fund)

Used for later phases of parking investments

Set aside for parking capital costs (e.g., a parking structure), particularly if on-street parking is eliminated in favor of transit or nonmotorized transportation modes

Used to support Transportation Demand Management programs, such as transit passes or Emery Go-Round service

Any other suggestions?

METERED PARKING

Goals

- 1. Increase parking turnover to make spaces available for customers
- 2. Provide alternatives for employee parking
- 3. Support Citywide goals of encouraging transit use, biking, and walking
- 4. Ensure financial sustainability for the parking program

Short-Term Metered

- Located in commercial areas visited heavily by customers who do not need to spend a long time at their destination

Mid-Term Metered

- Located in areas where some turnover is preferred, but not as much compared to short-term metered areas
- Business permits also available in North Bayfront and South of Powell neighborhoods for use at mid-term meters

Long-Term Metered

- Located in commercial areas where parking is used primarily by employees or long-term visitors, or in areas adjacent to visitor locations where employees can park for longer periods of time

Note: Parking regulations for permits and meters would be applicable Monday through Friday from 9am to 5pm.



Parking Duration and Pricing for Metered Parking

Hour	\$/Hour								\$ per day	\$ per week	\$ per month
	1	2	3	4	5	6	7	8			
Short-Term	\$2.00	\$2.00	\$7.00	\$7.00	\$7.00	\$7.00	\$7.00	\$7.00	\$46	\$230	\$920
Mid-Term	\$1.00	\$1.00	\$1.00	\$1.00	\$6.00	\$6.00	\$6.00	\$6.00	\$28	\$140	\$560
Long-Term	\$0.50	\$0.50	\$0.50	\$0.50	\$0.50	\$0.50	\$0.50	\$0.50	\$4	\$20	\$80

PERMITS

Goals: Residential Permits

1. Improve residents' ability to find parking close to home
2. Protect residents from spillover when meters are installed
3. Reduce commuter and special event parking in residential areas
4. Encourage people to use available off-street parking spaces
5. Incentivize employees and visitors to use other modes when possible

Goals: Business Permits

1. Support Emeryville businesses with limited on-site parking
2. Offer low-wage employees a cost effective alternative

North Hollis/Doyle Permit Area

Residents

- Limit of 2 permits per dwelling unit
- \$100/year for first permit and \$300/year for second permit*
- Visitor permit of \$5 per day

Businesses

- Total limit of 200 permits at \$200/year per permit*

Triangle Permit Area

Residents

- Limit of 2 permits per dwelling unit
- \$100/year for first permit and \$300/year for second permit*
- Visitor permit of \$5 per day

Businesses

- Total limit of 100 permits at \$200/year per permit*

Mid-Term Metered Areas

- Businesses may also be issued permits to use in the mid-term metered parking areas in the North Bayfront and South of Powell neighborhoods only

* Pending further analysis

Note: Parking regulations for permits and meters would be applicable Monday through Friday from 9am to 5pm.

